



COUNTY ROAD 53 IMPROVEMENTS

County Road 21 to City of Barrie Limits
Class Environmental Assessment

PUBLIC ENGAGEMENT



April 27, 2023 – May 25, 2023

1

**STUDY PURPOSE &
PROCESS**

2

**PROBLEM OR
OPPORTUNITY**

3

**ALTERNATIVE SOLUTIONS
& RECOMMENDATIONS**

4

NEXT STEPS

**AG
G
E
N
D
A**



1

**STUDY PURPOSE
& PROCESS**

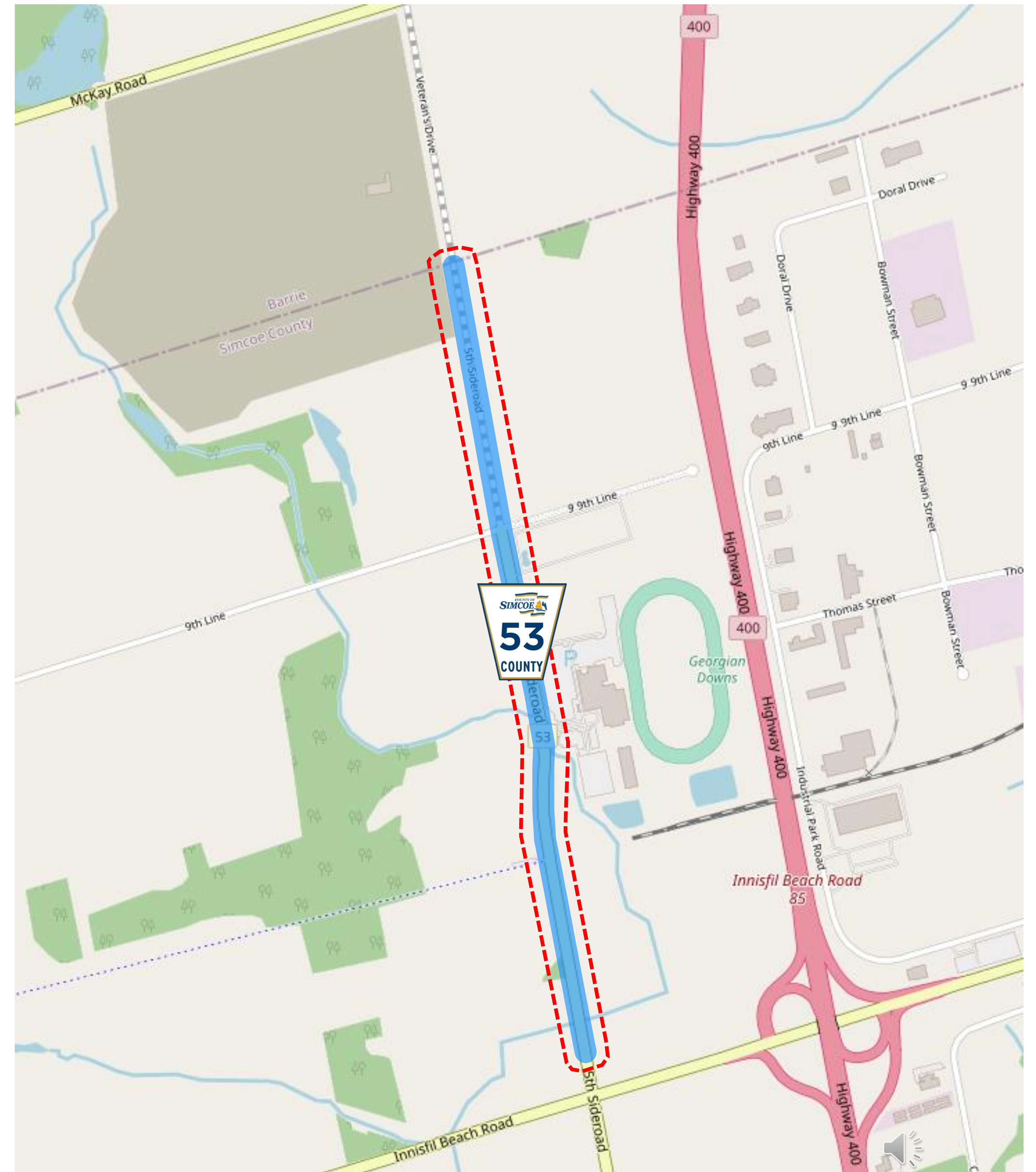


STUDY AREA

- County Road 21 to City of Barrie limits

BACKGROUND

- County Road 53 (formerly 5th Sideroad) was transferred to the County in 2011
- The *2014 County of Simcoe Transportation Master Plan (TMP) Update* identified the need for increased capacity along the road
- As a former Town road, County Road 53 requires upgrading to County standards



The **OBJECTIVE** of this study is to:

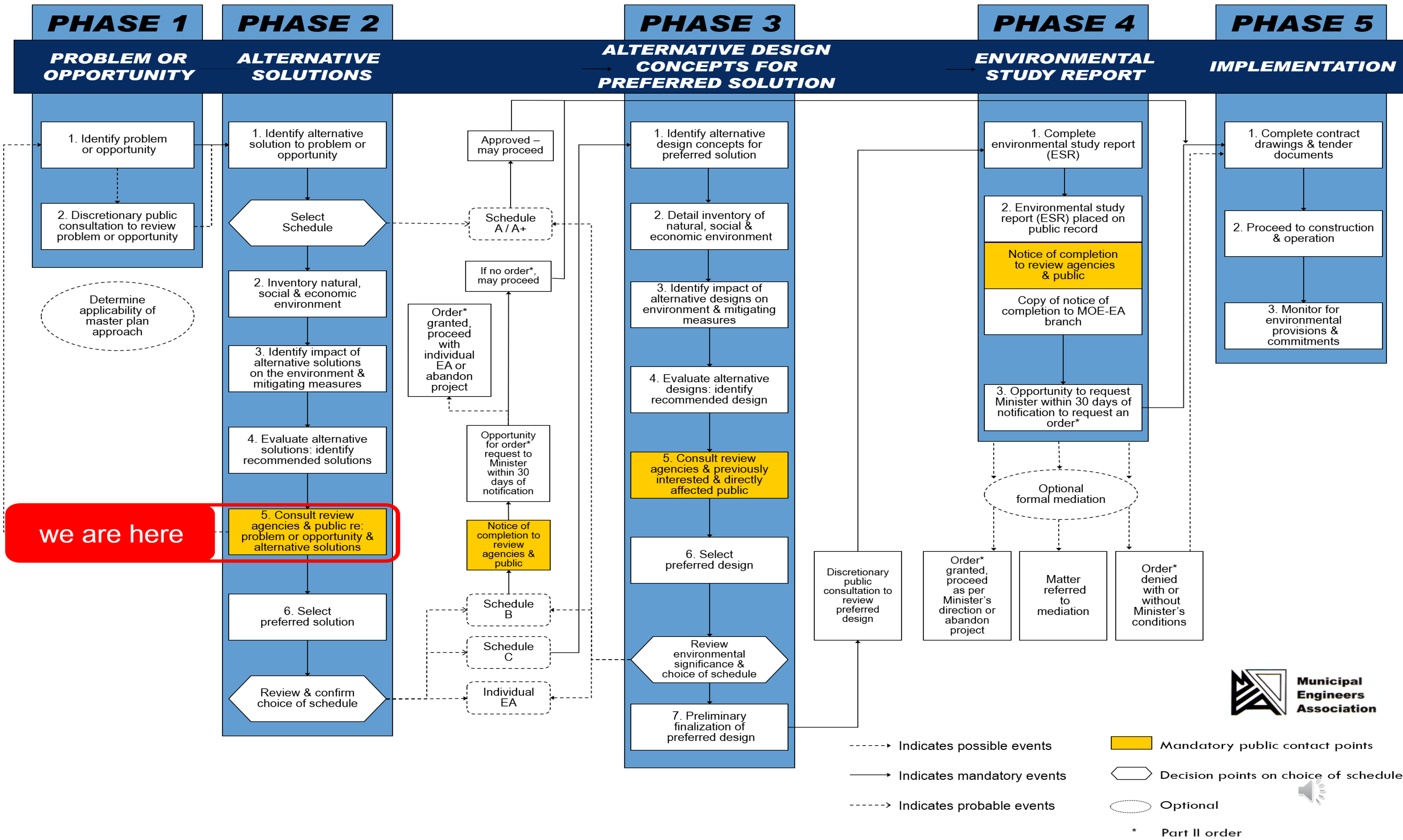
- identify and facilitate the implementation of improvements to County Road 53 in consideration of
 - the natural, socio-economic & heritage environments
 - the intended function of a county road
 - the needs of road users
 - the County's current road standards



The **PURPOSE** of this study is to:

- develop alternative solutions to accommodate future traffic demands
- identify the location, extent and sensitivity of affected environments
- assess the alternatives given potential environmental impacts
- identify a preferred solution
- establish measures to mitigate impacts
- satisfy the Class EA requirements





County Road 53

Improvements: Schedule B

WE ARE HERE



Notice of
Commencement



Public
Engagement

Phase 1

Identify & describe the problem or opportunity

Schedules
A, A+, B & C

Phase 2

Identify & assess alternative solutions to solve the problem

Establish the Preferred Solution

Confirm Class EA Schedule

Schedules
B & C

Phase 3

Identify & assess alternative designs for the Preferred Solution

Establish the Preferred Design

Schedule
C

Phase 4

Prepare an Environmental Study Report

Schedule
C

Phase 5

Design & Construction

Schedules
A, A+, B & C

The purpose of **PUBLIC ENGAGEMENT** is to:

- initiate the public engagement process
- identify the study area, study objective and purpose
- present the need and justification for the study
- identify the alternative solutions and potential environmental impacts
- seek input and comments for consideration in the selection of the preferred options



2

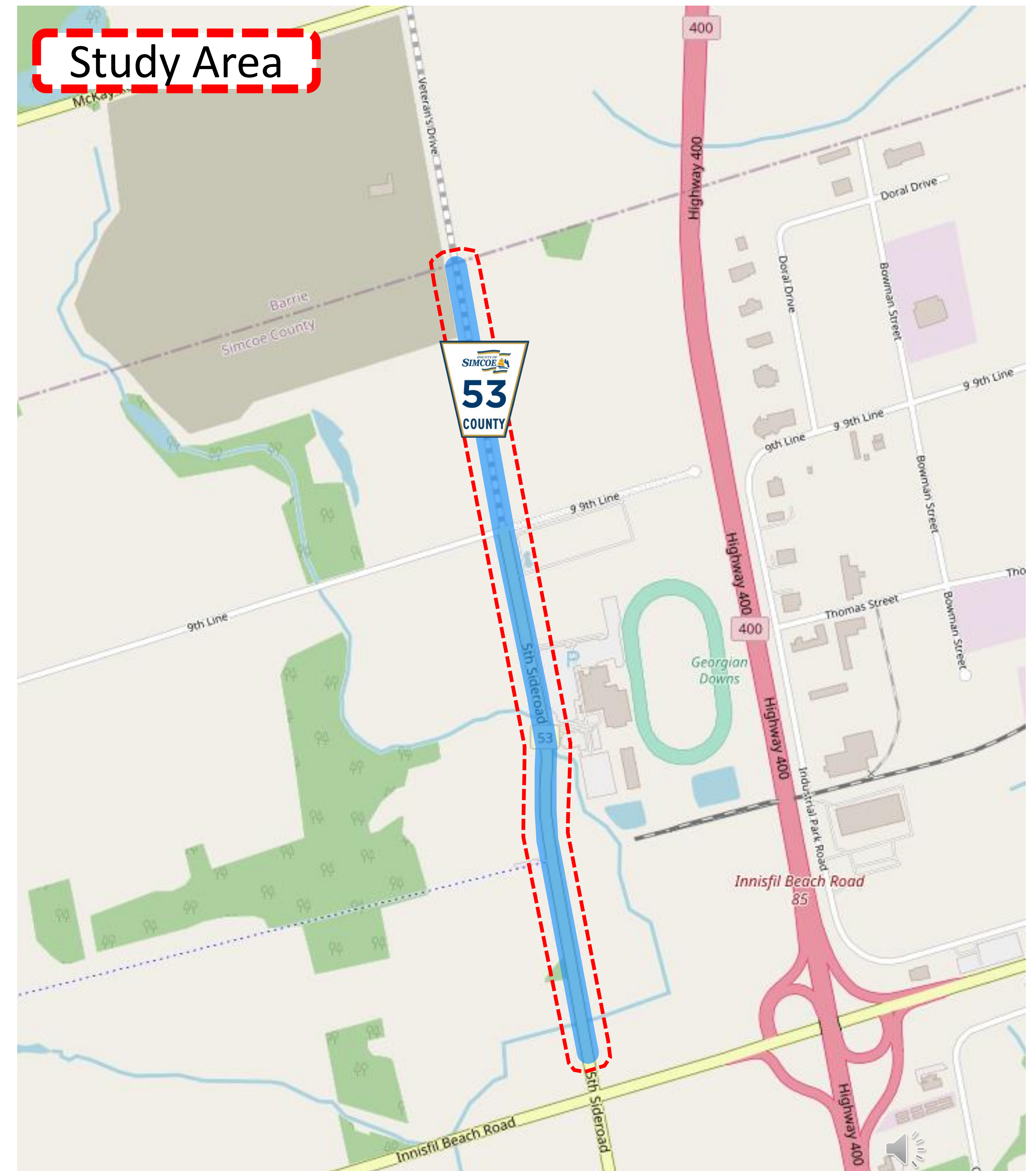
PHASE 1

PROBLEM OR OPPORTUNITY



BACKGROUND

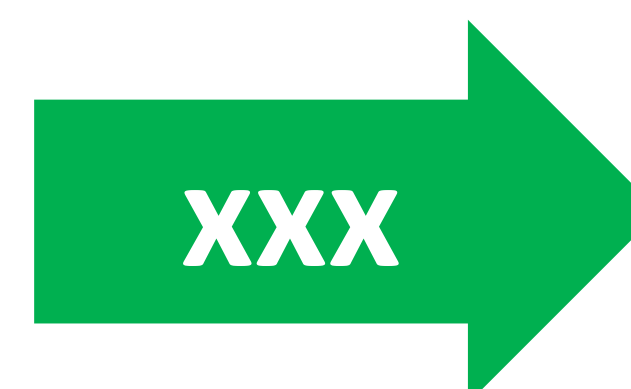
- Transferred to the County in 2011 and does not currently satisfy County road standards
- County of Simcoe Official Plan
 - ROW width of 40 metres identified
 - existing ROW varies between 20 and 54 metres, with a predominant ROW of 26 metres
- County of Simcoe TMP Update (2014)
 - recommended additional capacity



TRAFFIC OPERATIONS

- Traffic Operations Assessment
 - traffic projections established for 2030 and 2040 horizons
 - consideration given to impacts associated with proposed Hwy 400/McKay Road interchange
 - volumes exceed available capacity by 2040
 - intersection operations fail by 2040

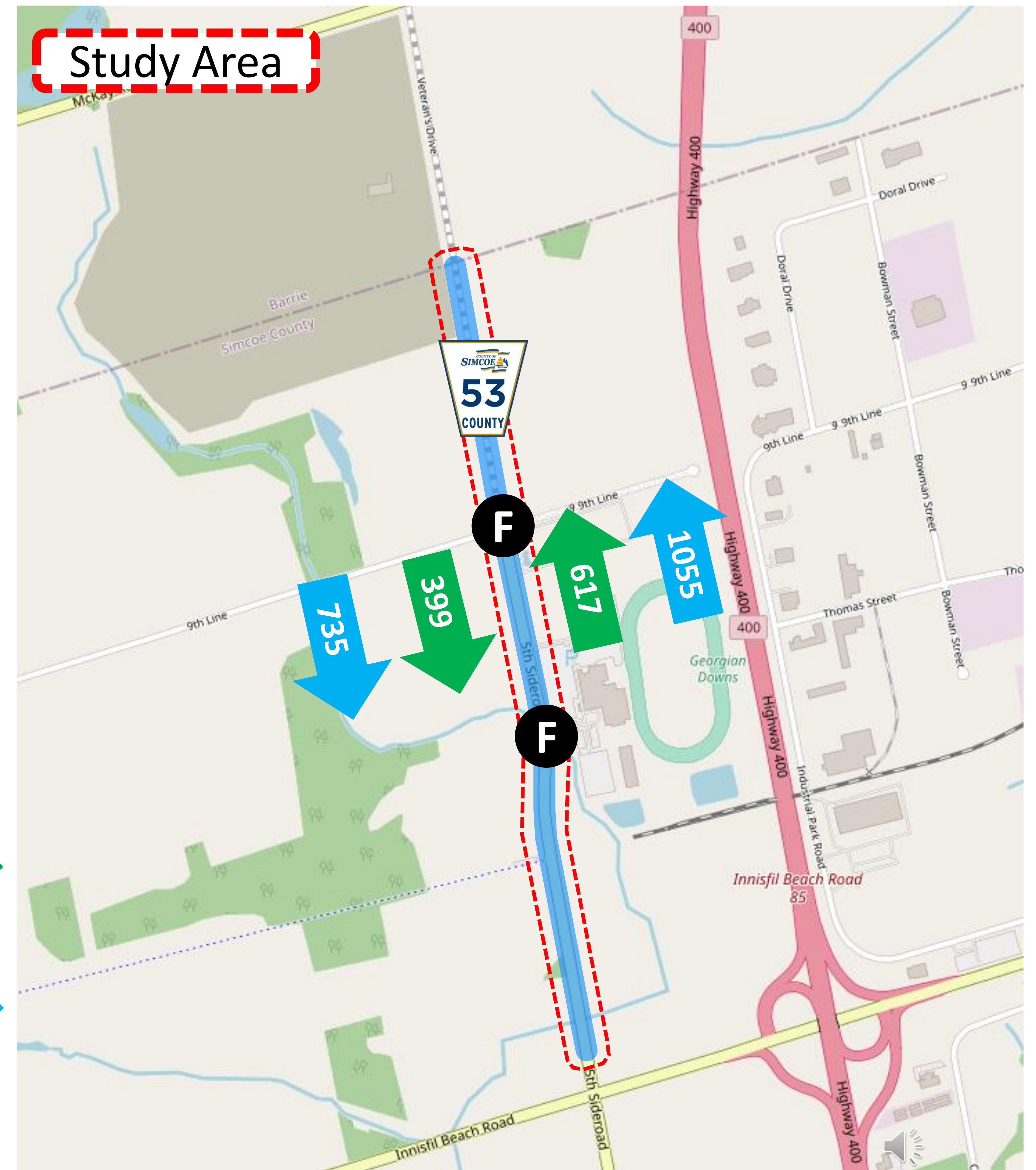
hourly traffic volumes - Year 2019



hourly traffic volumes - Year 2040



hourly road capacity
vehicles per hour per lane (vphpl)



That improvements necessary to support the intended arterial function of County Road 53 be addressed in an environmentally sound manner in consideration of:

- future traffic needs
- current County standards
- surface drainage requirements

The overall intent being the delivery of a County road facility that provides

SAFE and **EFFICIENT**

travel for its users.



3

PHASE 2

ALTERNATIVE SOLUTIONS



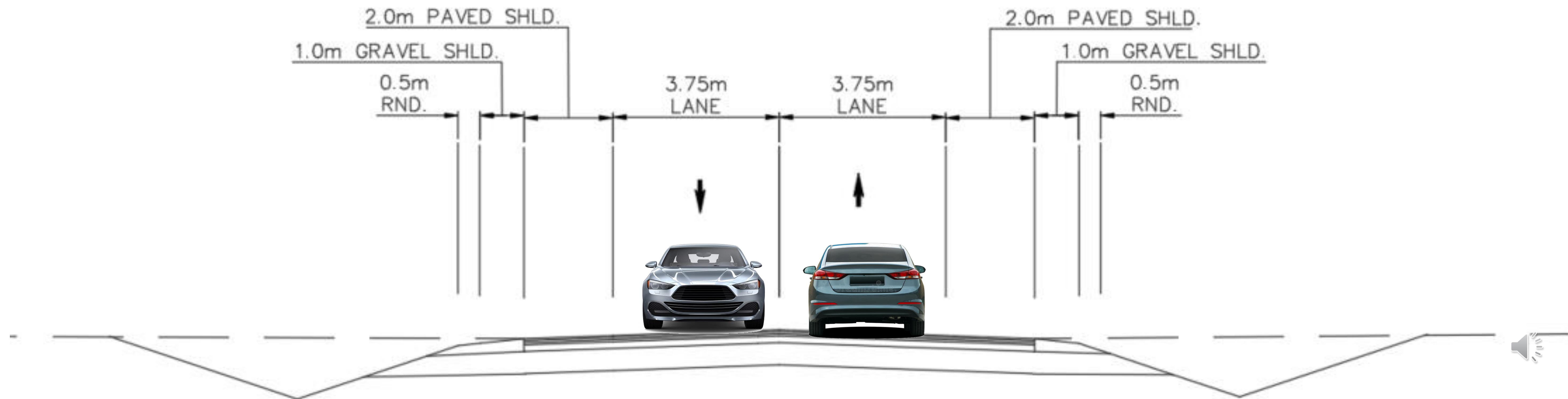
DO NOTHING

- maintain existing conditions with no improvements
- 3.5 metre lanes and 1.5 metre shoulders

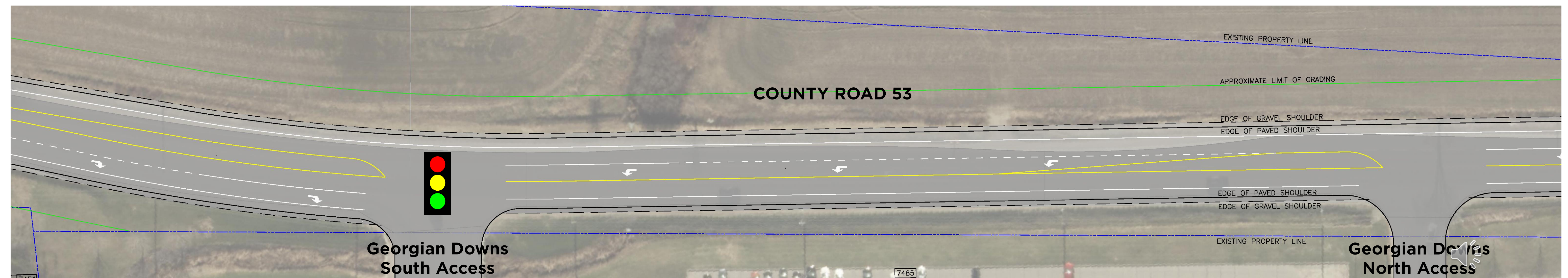
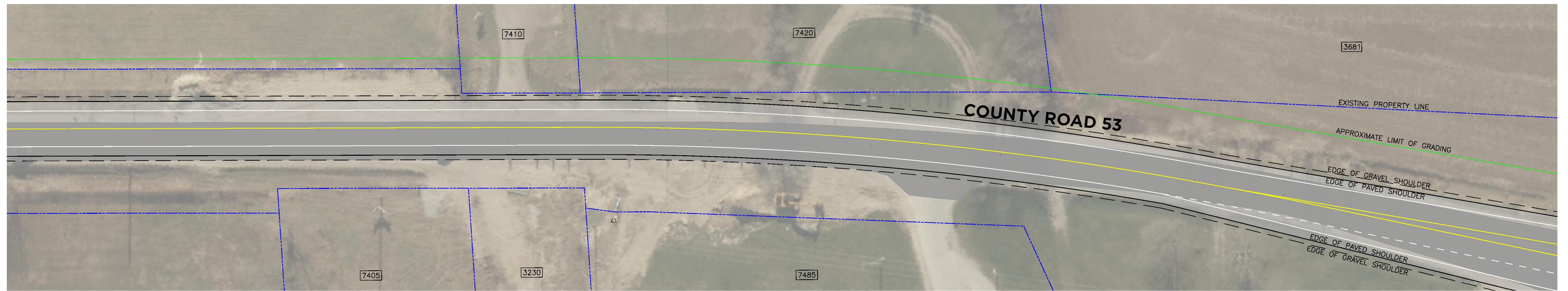
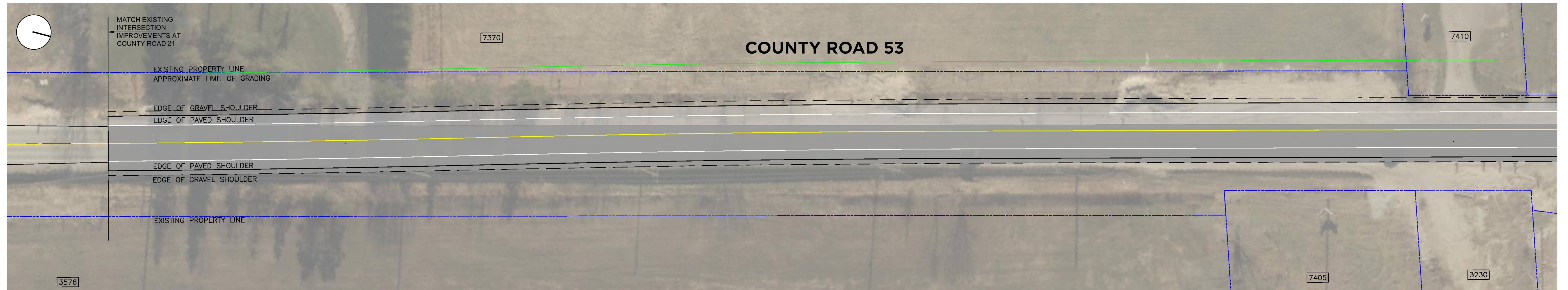


ALTERNATIVE A: OPERATIONAL IMPROVEMENTS

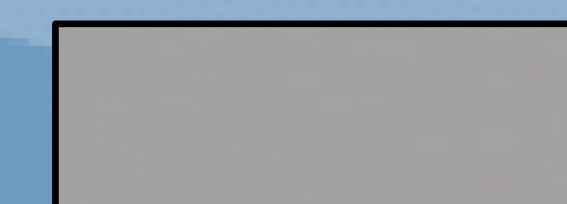
- maintain 2-lane cross-section with intersection improvements (turn lanes and traffic signals)
- upgrade cross-section to County standard (wider lanes and wider shoulders)
- hold east edge of shoulder
- widen ROW to the west as needed to accommodate intersection improvements and improved drainage (widening to east constrained due to Innisvale Cemetery, buried gas main, and hydro poles)



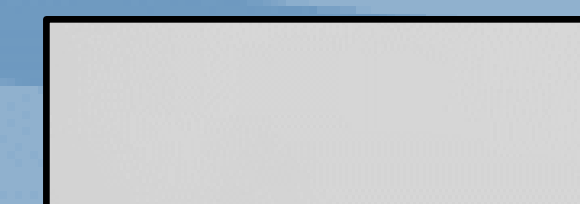
ALTERNATIVE A: OPERATIONAL IMPROVEMENTS



ALTERNATIVE SOLUTIONS

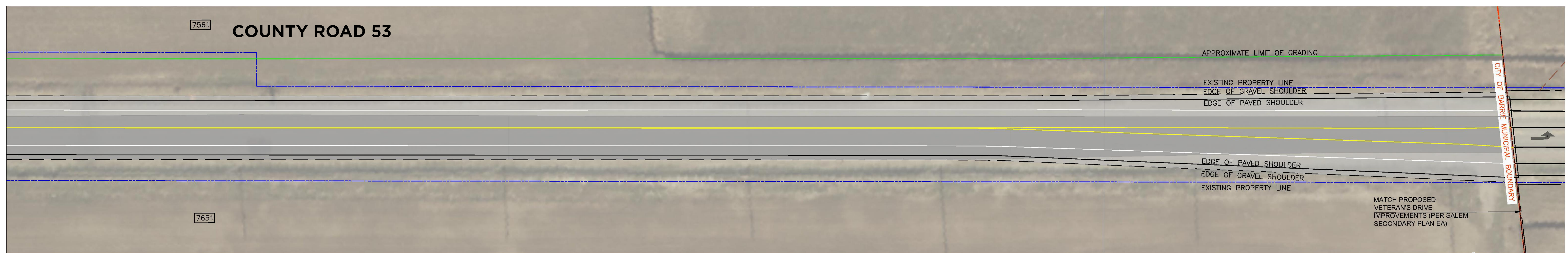
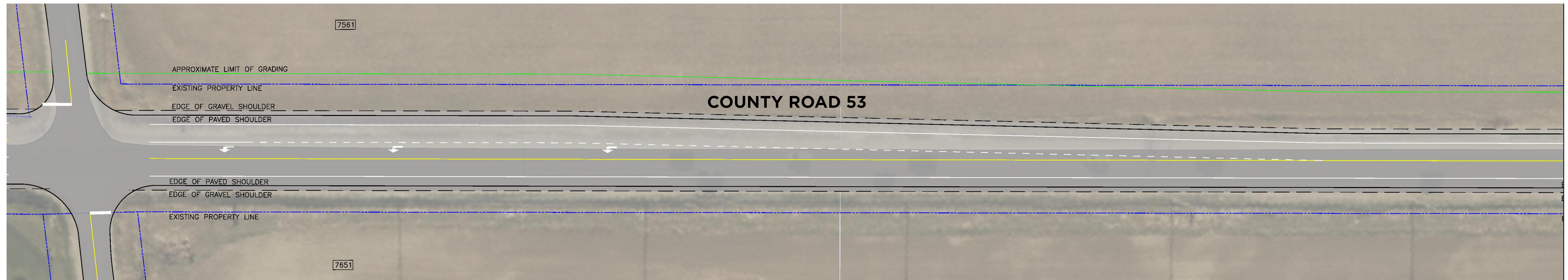
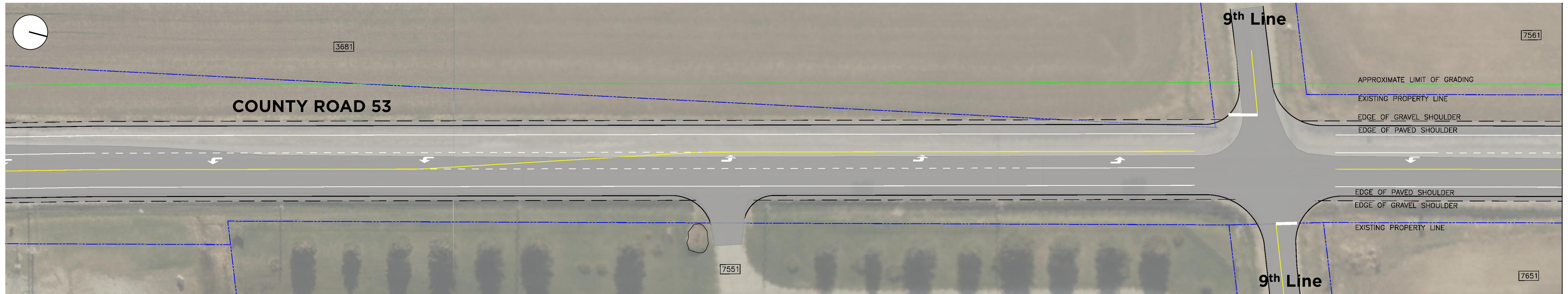


Limit of existing road platform

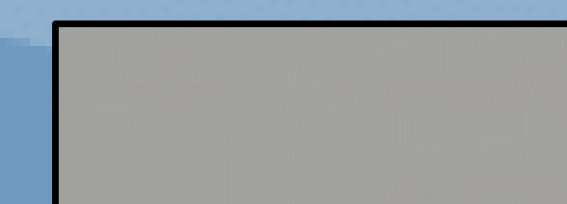


Limit of proposed widening

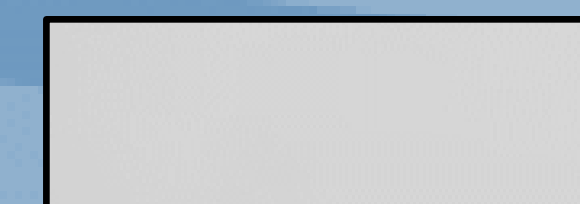
ALTERNATIVE A: OPERATIONAL IMPROVEMENTS



ALTERNATIVE SOLUTIONS



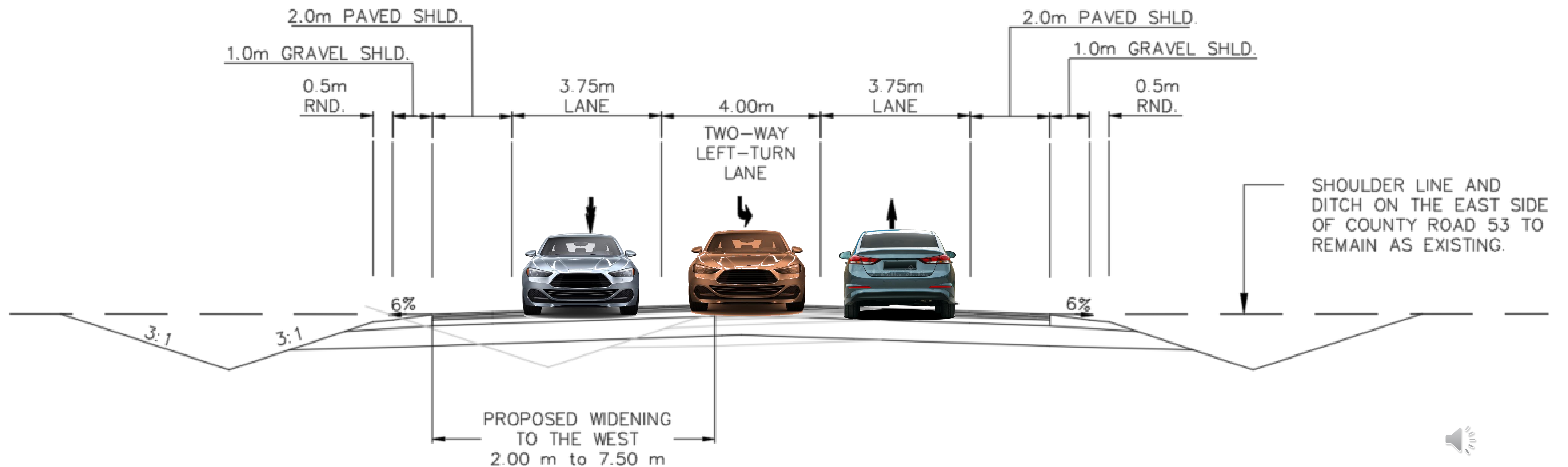
Limit of existing road platform



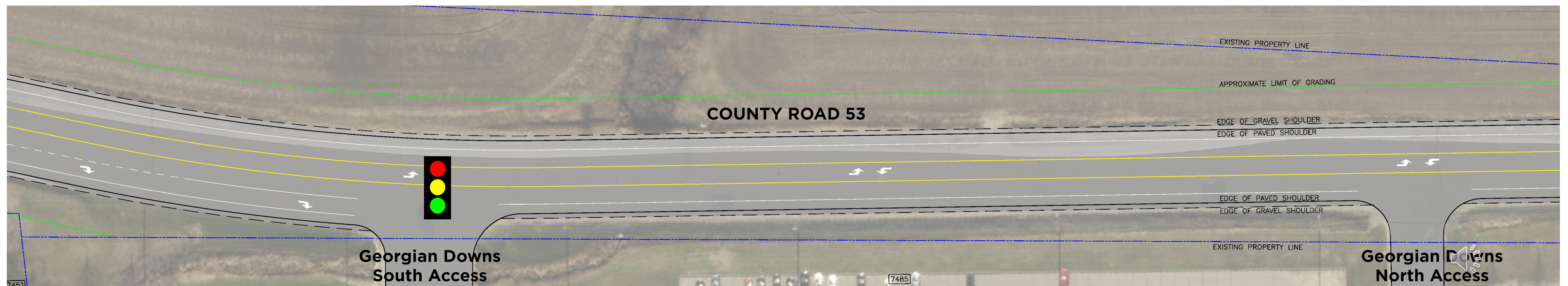
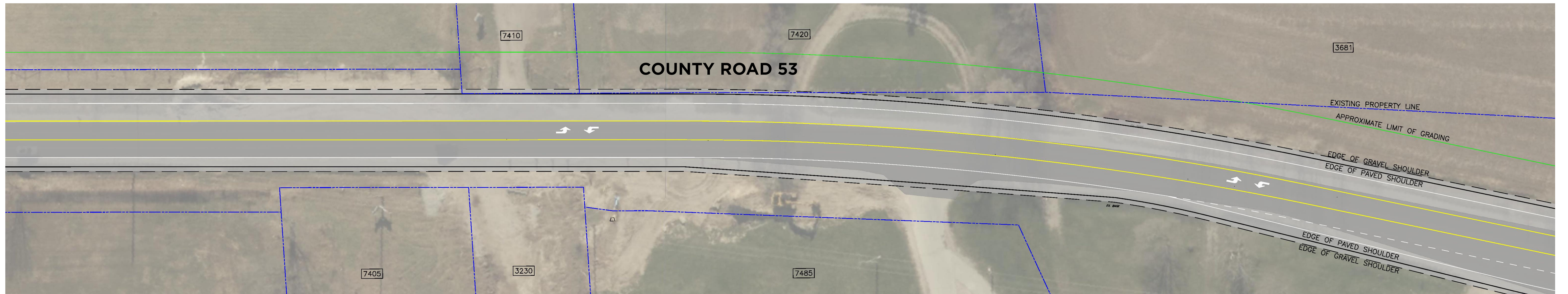
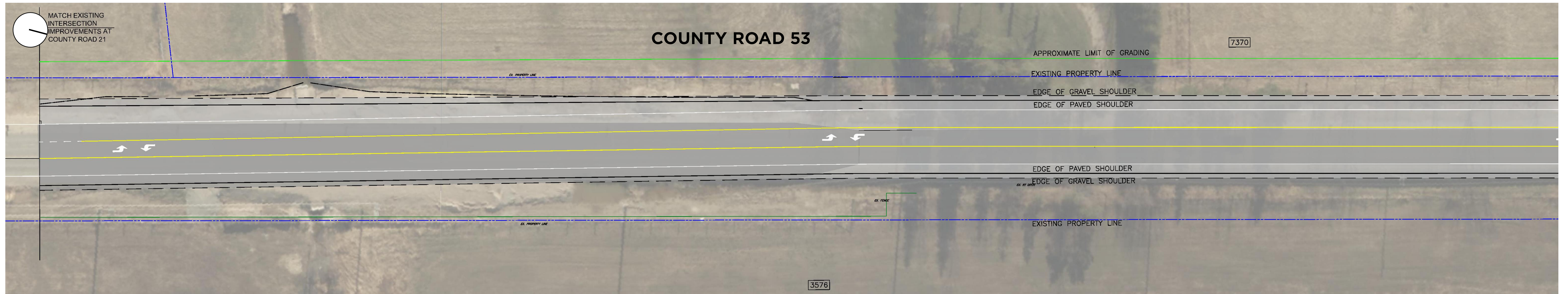
Limit of proposed widening

ALTERNATIVE B: WIDEN TO 3 LANES

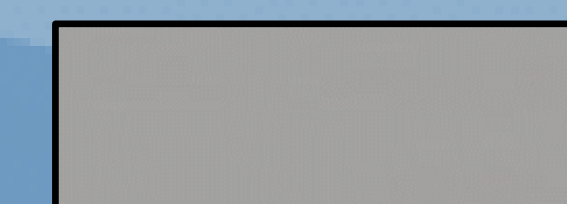
- widen road to 3-lane cross-section to increase capacity (1 lane per direction + centre turn lane)
- improve lane configurations at intersections with traffic signals as needed
- widen ROW to the west as needed to accommodate 3-lanes and improved drainage
(widening to east constrained due to Innisvale Cemetery, buried gas main, and hydro poles)



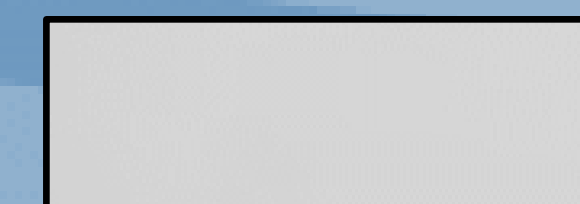
ALTERNATIVE B: WIDEN TO 3 LANES



ALTERNATIVE SOLUTIONS

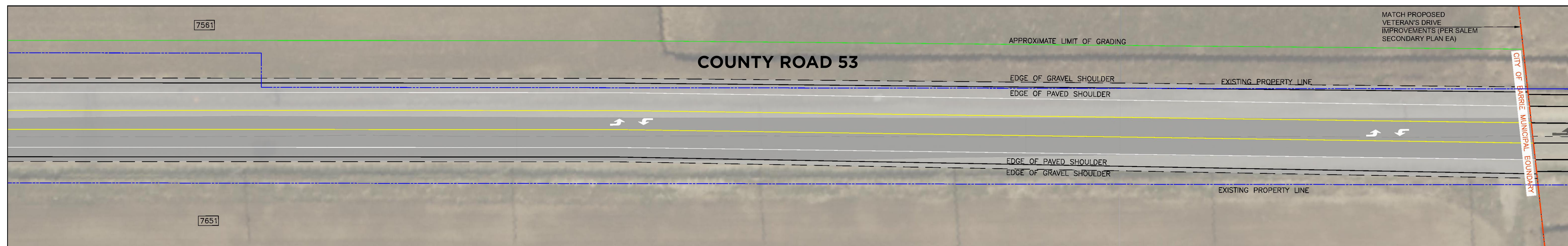
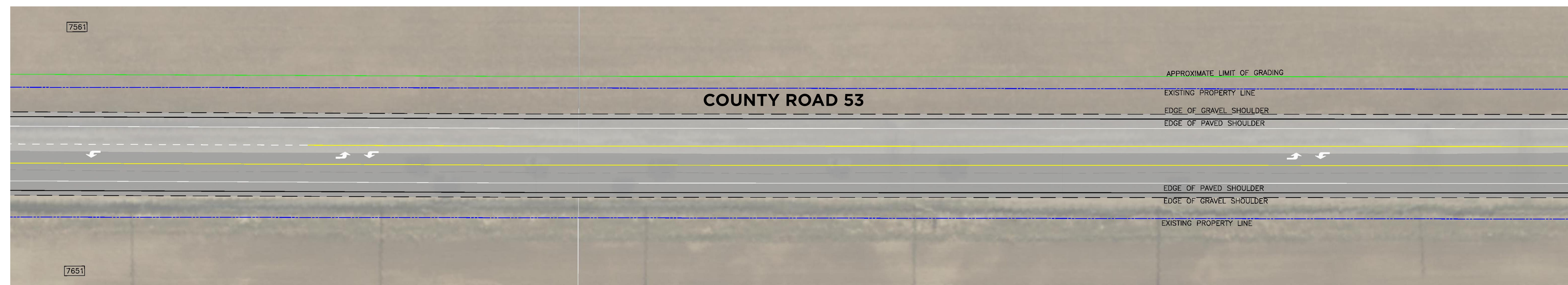
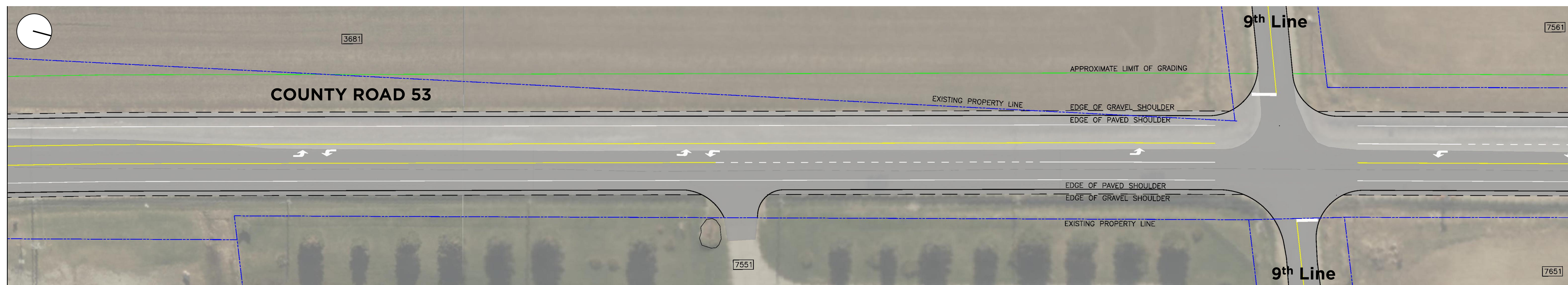


Limit of existing road platform

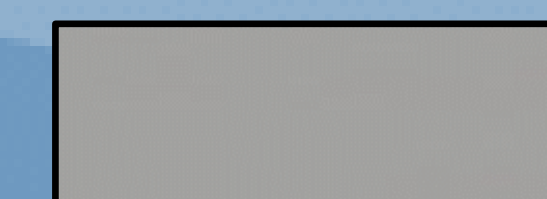


Limit of proposed widening

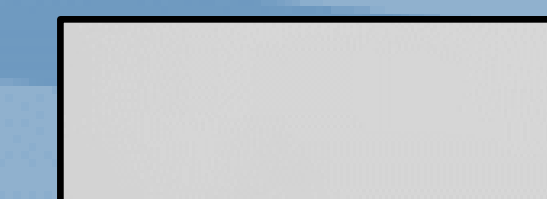
ALTERNATIVE B: WIDEN TO 3 LANES



ALTERNATIVE SOLUTIONS



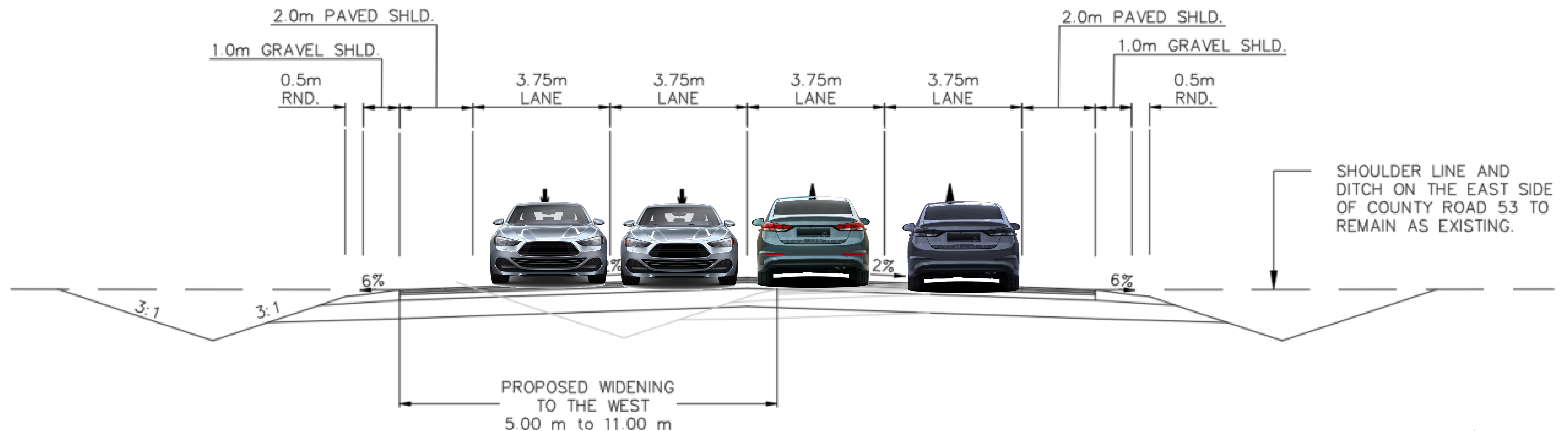
Limit of existing road platform



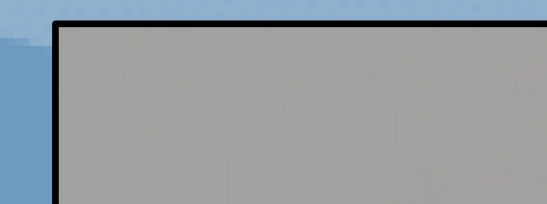
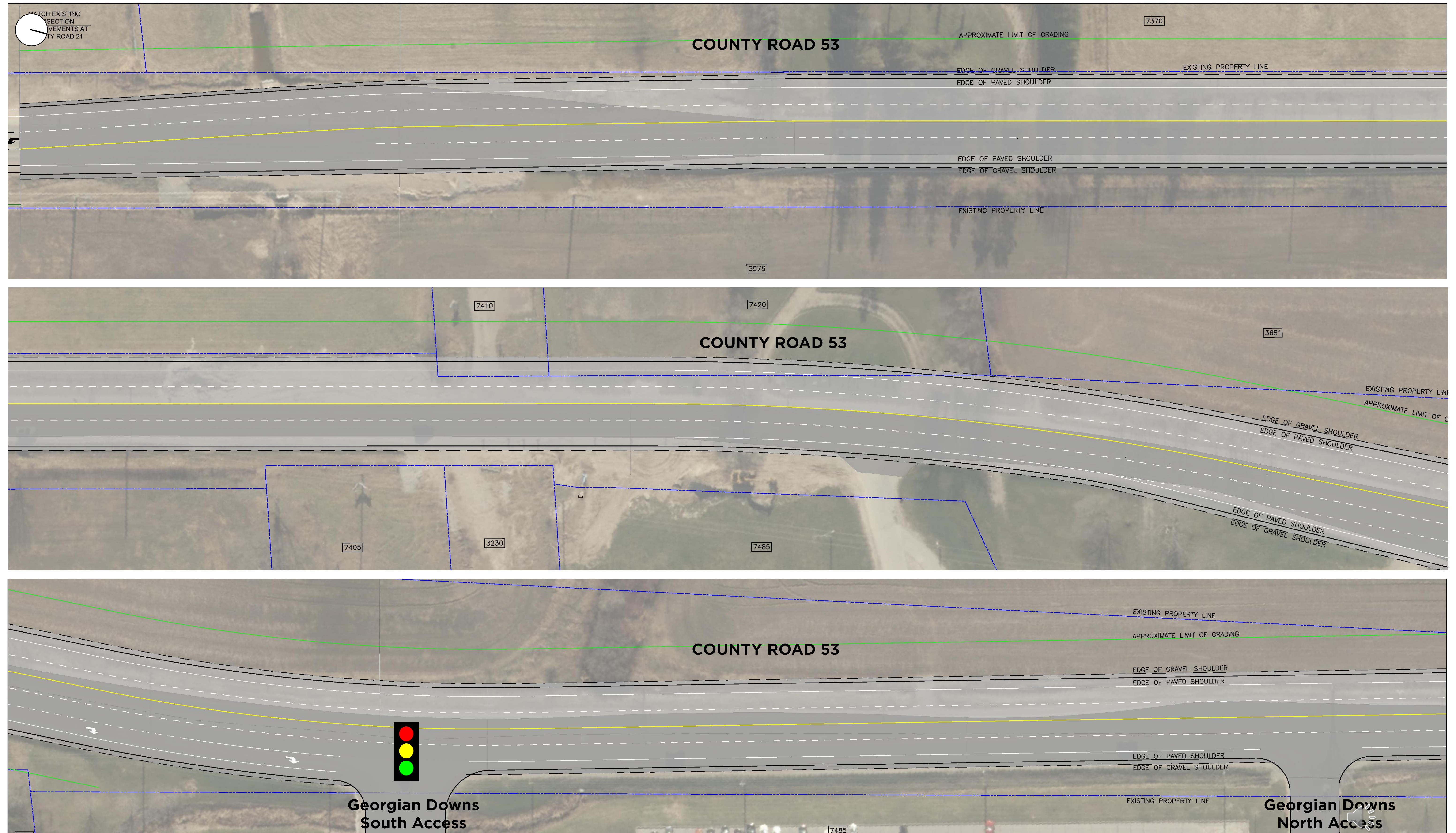
Limit of proposed widening

ALTERNATIVE C: WIDEN TO 4 LANES

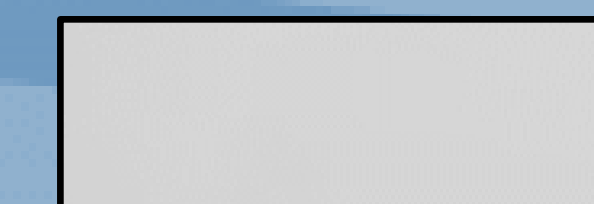
- widen road to 4-lane cross-section to increase capacity
(2 lanes per direction, no centre turn lane)
- widen ROW to west as required to accommodate 4-lane cross section and improved drainage
(widening to east constrained due to Innisvale Cemetery, buried gas main, and hydro poles)



ALTERNATIVE C: WIDEN TO 4 LANES

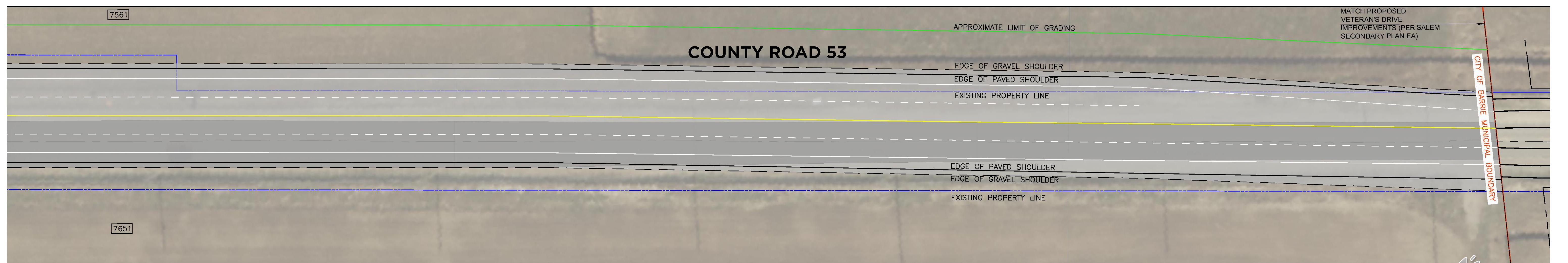
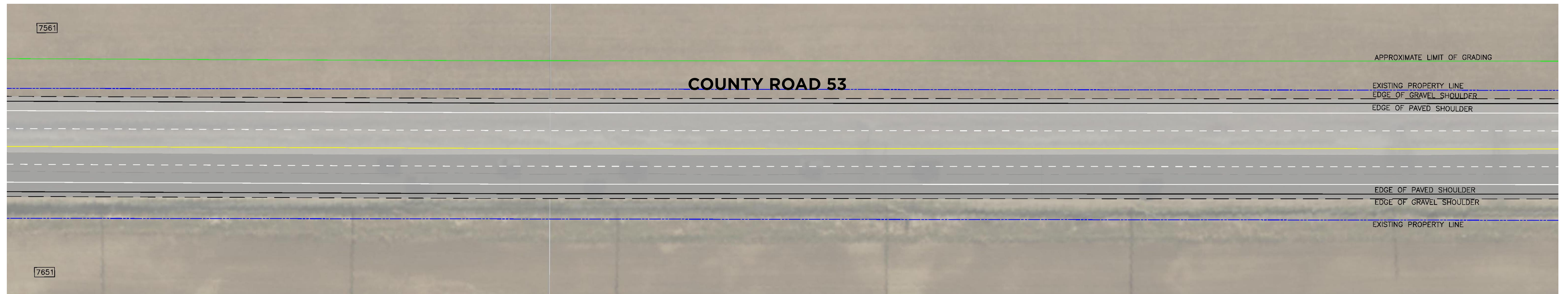
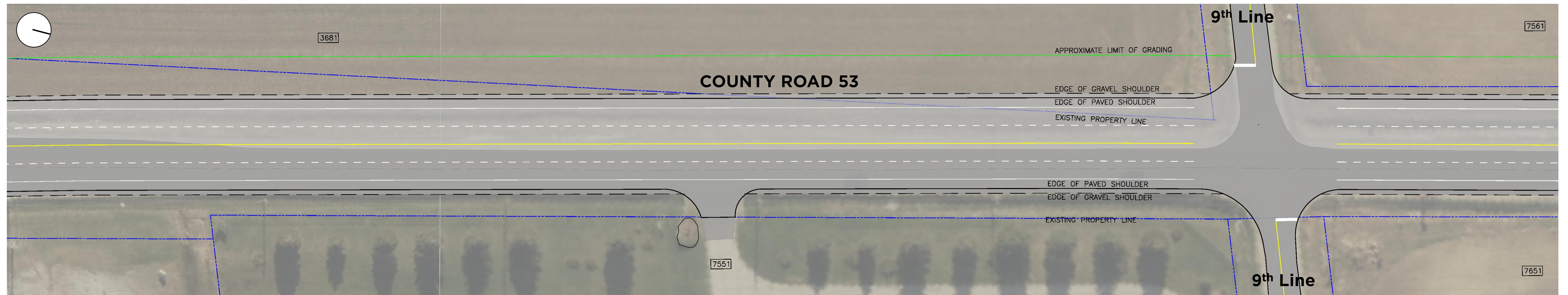


Limit of existing road platform

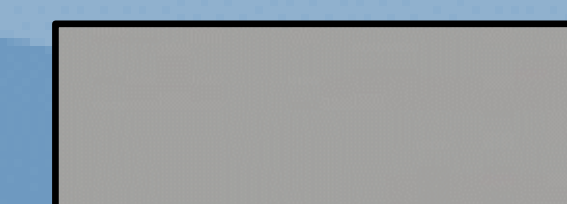


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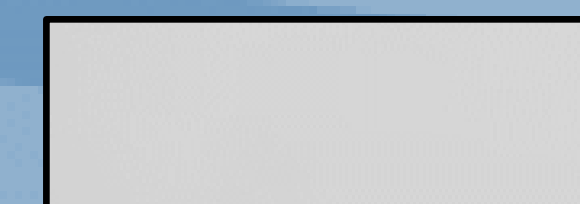
ALTERNATIVE C: WIDEN TO 4 LANES



ALTERNATIVE SOLUTIONS



Limit of existing road platform

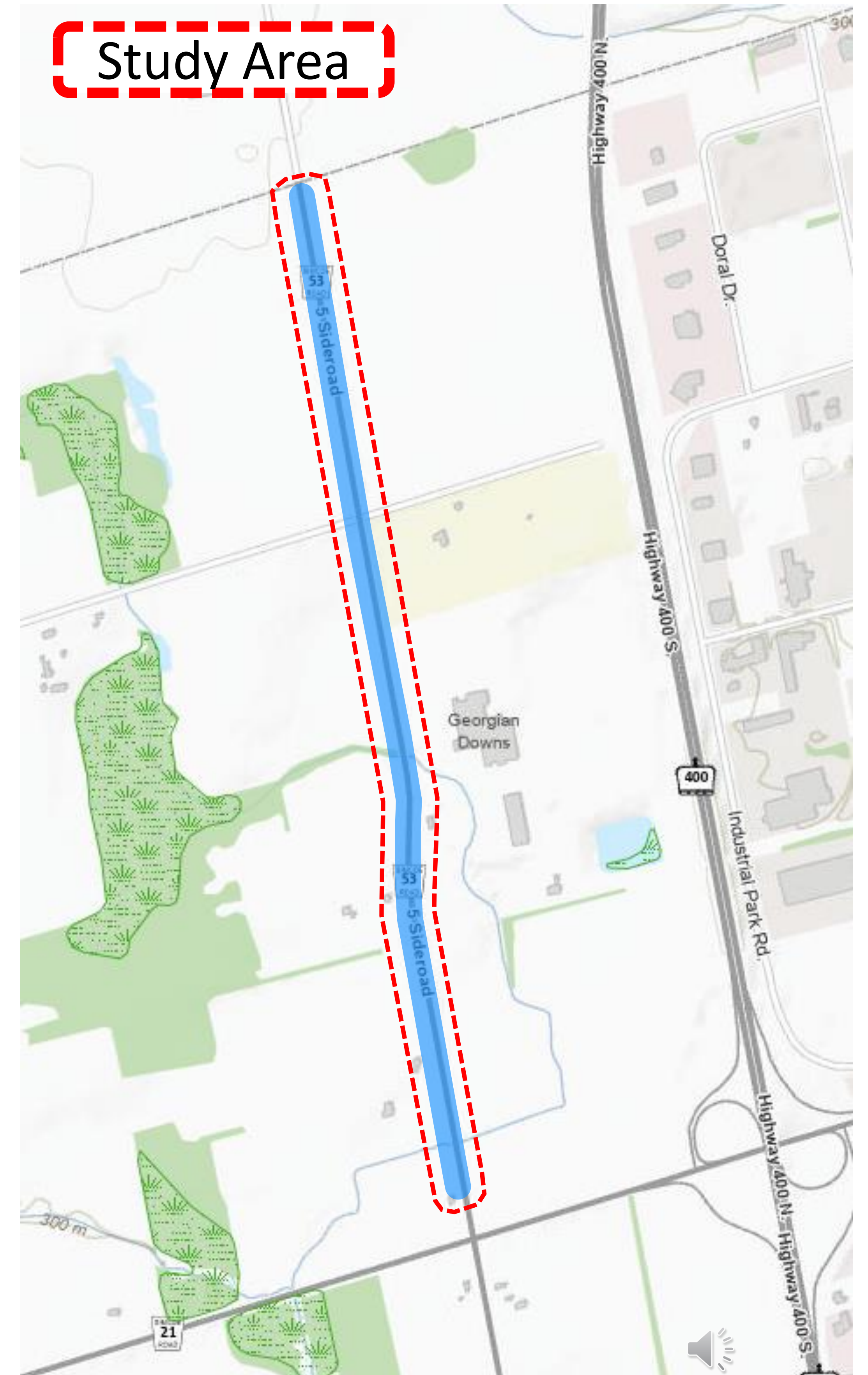


Limit of proposed widening

NATURAL ENVIRONMENT

- Study area consists of active agricultural lands, residential lots, Gateway Casino and Innisvale Cemetery/Crematorium
- Key Natural Heritage Features (KNHF) include:
 - wetland & woodland
 - significant wildlife habitat
 - fish habitat
 - habitat for endangered and threatened species
- No significant valleyland or Areas of Natural and Scientific Interest within study area

**Mitigate impacts through
Best Management Practices**



CULTURAL HERITAGE

- Cultural Heritage Assessment Report identified the following for further investigation:
 - Built Heritage Resource - 7370 County Road 53
 - Cultural Heritage Landscape - Thornton Cookstown Trans-Canada Trail
- Cultural Heritage Impact Assessment concluded the following:
 - limited anticipated adverse impacts to heritage attributes at 7370 County Road 53
 - potential for loss of mature trees
 - no anticipated adverse impacts to Thornton Cookstown Trans Canada Trail.

Best Management Practices

recommended to mitigate impact to mature trees at 7370 County Road 53.



ARCHAEOLOGICAL

- assessment based on current Stage I Archaeological Assessment and historical Stage I and II Archaeological Assessments conducted along the study area
- portions of study area identified as having no or low archaeological potential and are exempt from requiring additional investigation
- areas identified as retaining archaeological potential are subject to Stage II investigation at detail design and prior to construction

No known impacts

Stage II assessment required at detail design for areas identified as retaining archaeological potential.

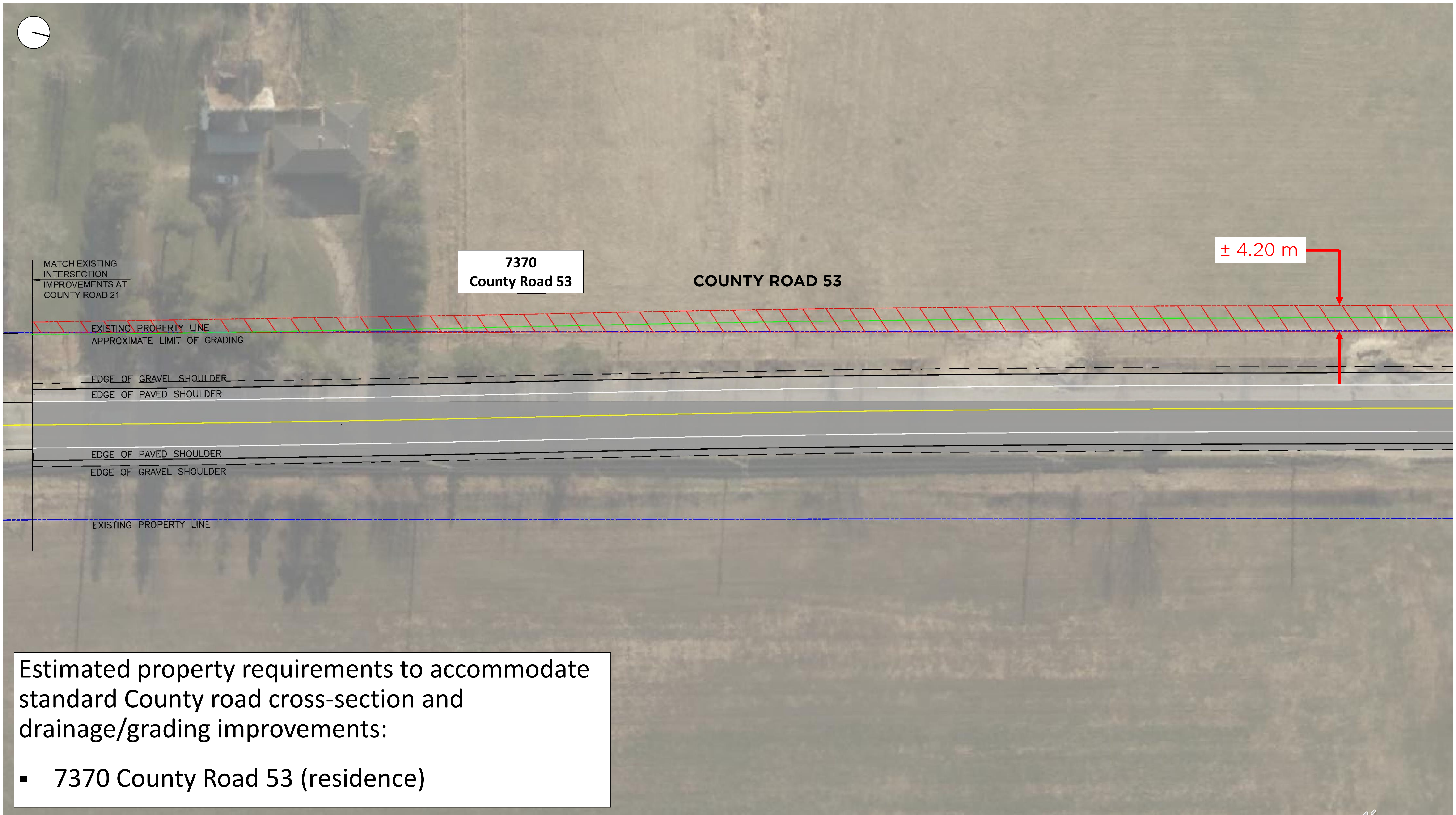


ASSESSMENT CRITERIA	Alternative A Operational Improvements	Alternative B Widen to 3-Lanes	Alternative C Widen to 4-Lanes
Natural Environment			
Cultural Heritage/Archaeological			
Social Environment			
Economic Environment			
Transportation Needs			
RECOMMENDATION	Alternative A – Operational Improvements		

 GOOD

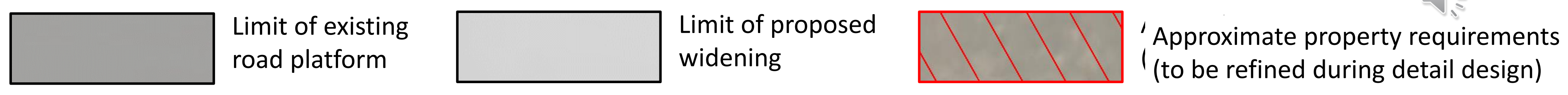
 BETTER

 BEST 

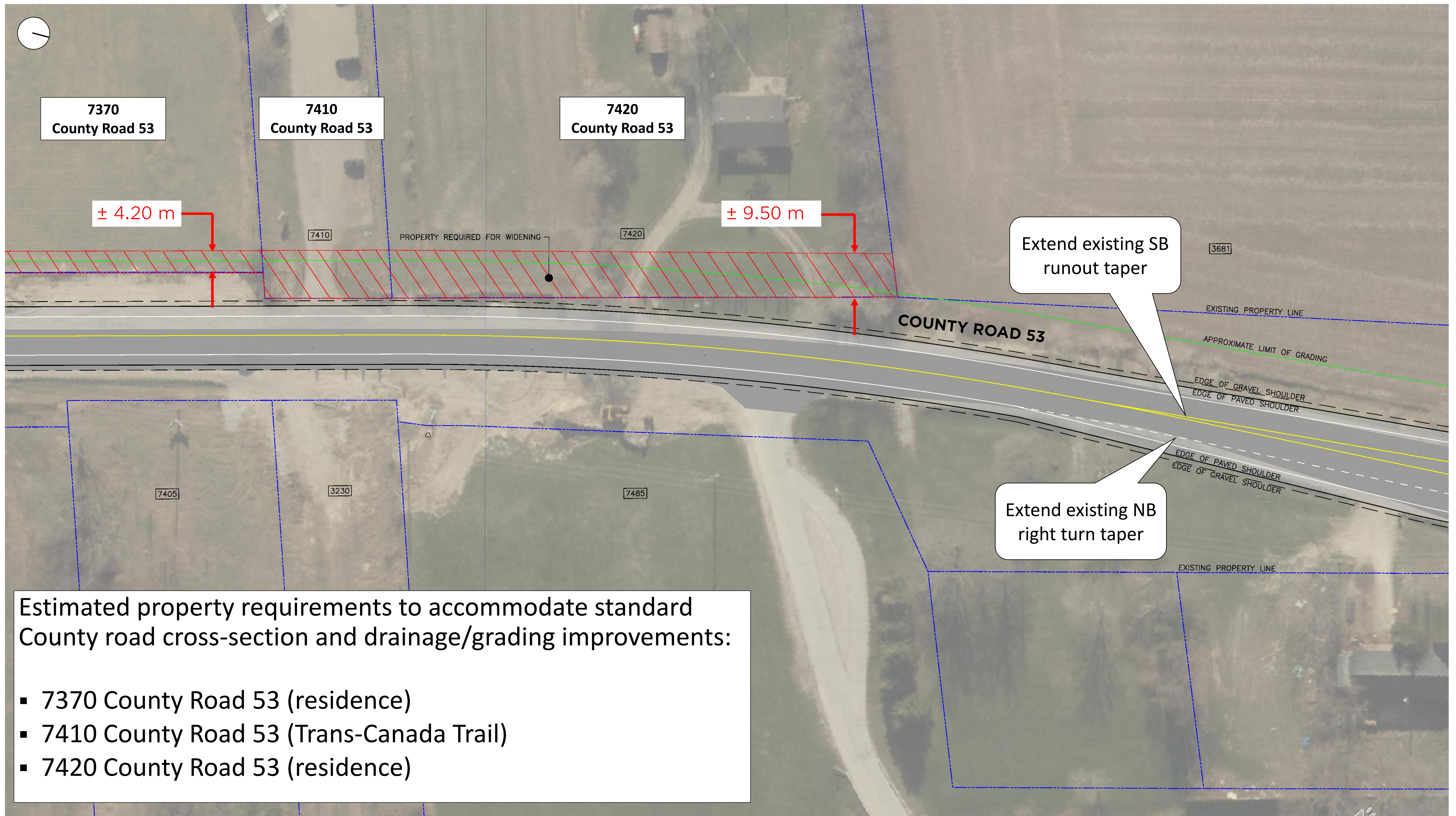


Estimated property requirements to accommodate standard County road cross-section and drainage/grading improvements:

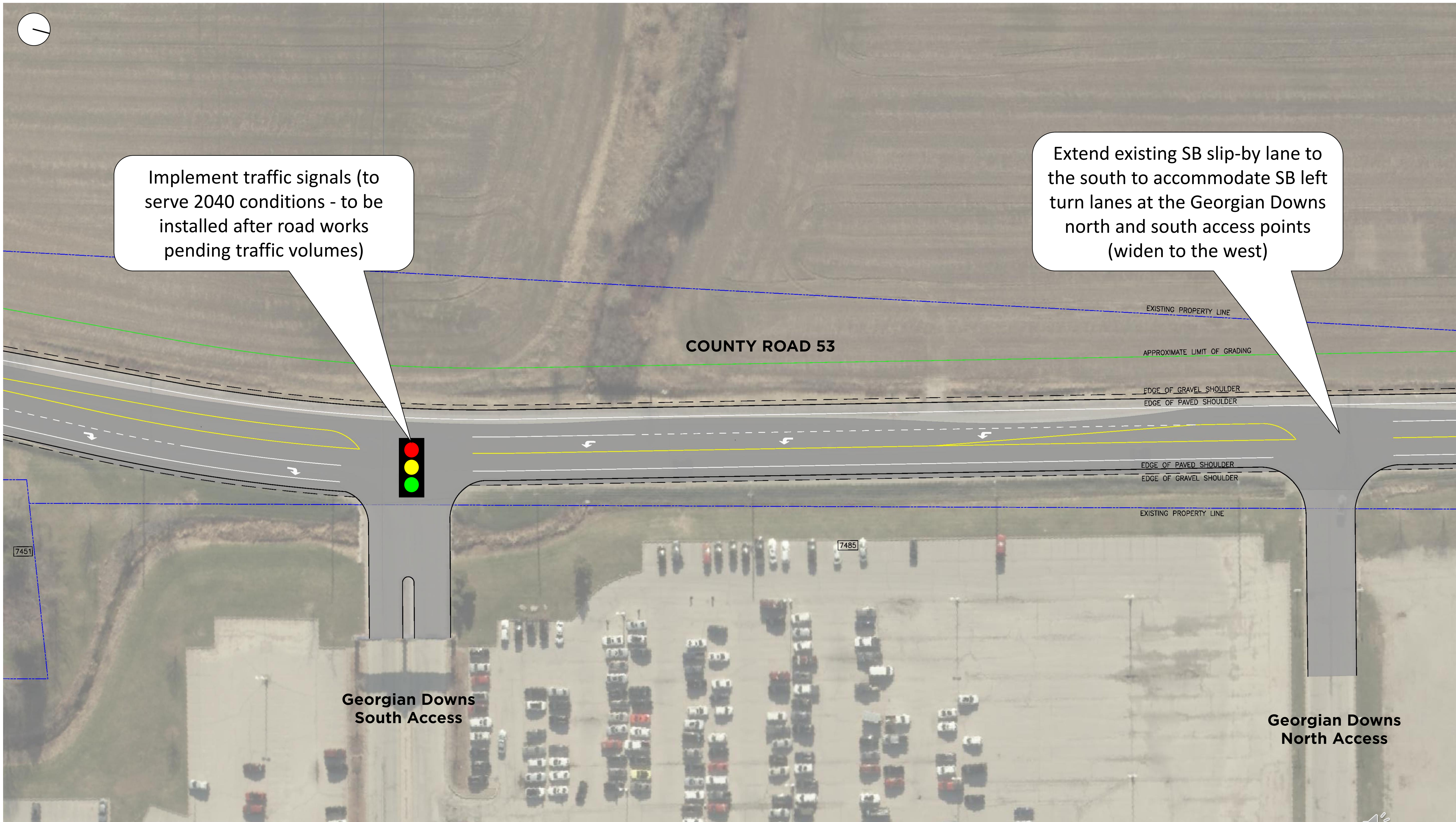
- 7370 County Road 53 (residence)



RECOMMENDED SOLUTION – PROPERTY IMPACTS



RECOMMENDED SOLUTION – PROPERTY IMPACTS



Implement traffic signals (to serve 2040 conditions - to be installed after road works pending traffic volumes)

Extend existing SB slip-by lane to the south to accommodate SB left turn lanes at the Georgian Downs north and south access points (widen to the west)

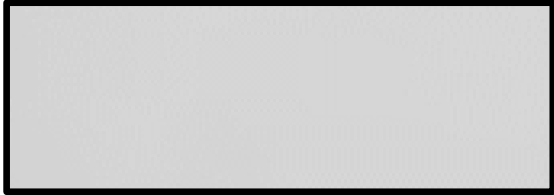
COUNTY ROAD 53

Georgian Downs South Access

Georgian Downs North Access

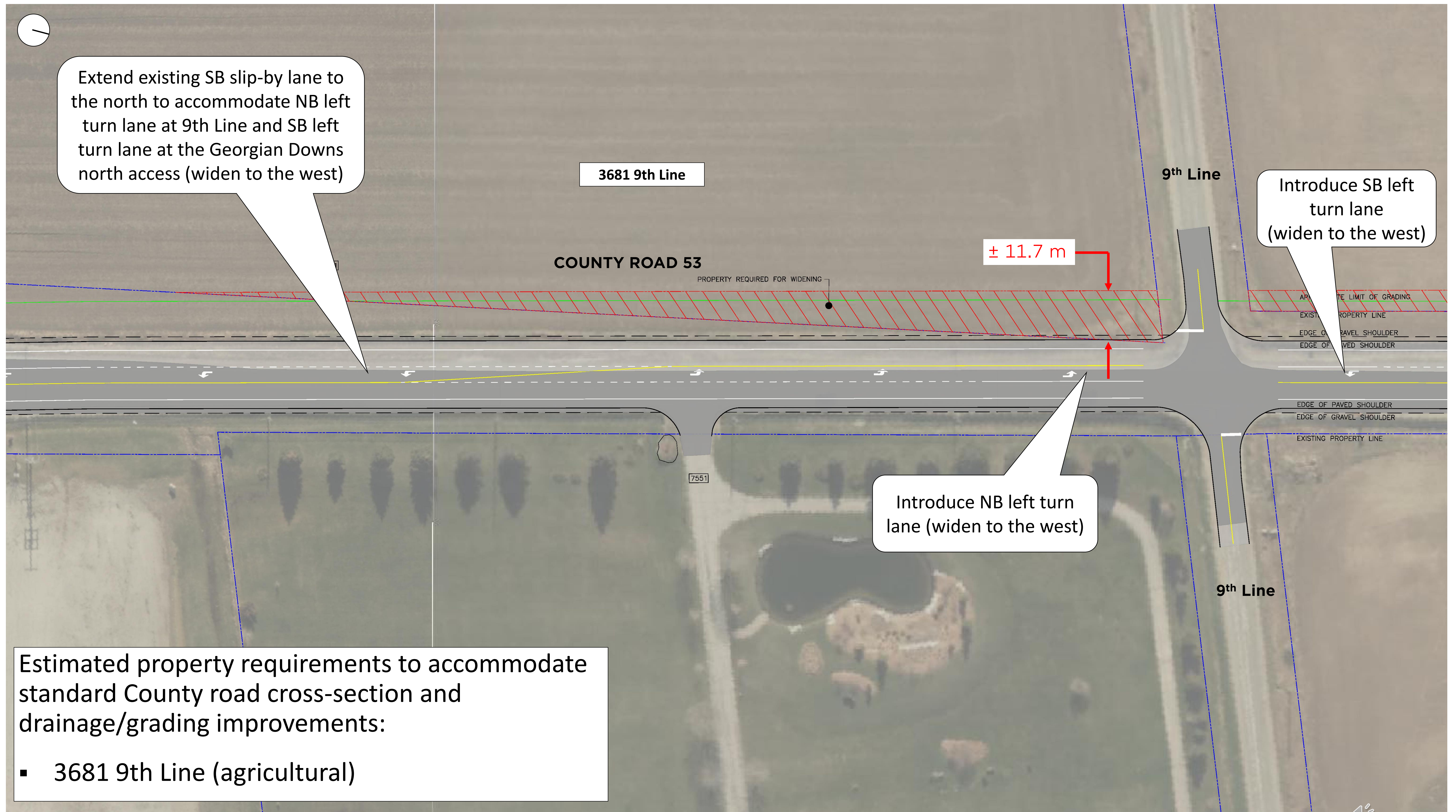


Limit of existing road platform



Limit of proposed widening

RECOMMENDED SOLUTION – PROPERTY IMPACTS



Extend existing SB slip-by lane to the north to accommodate NB left turn lane at 9th Line and SB left turn lane at the Georgian Downs north access (widen to the west)

3681 9th Line

9th Line

Introduce SB left turn lane (widen to the west)

COUNTY ROAD 53

± 11.7 m

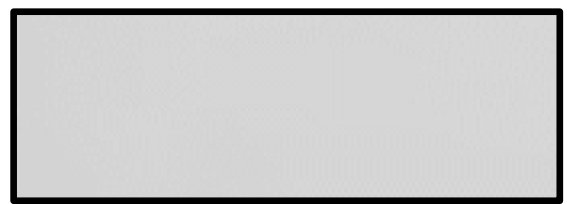
Introduce NB left turn lane (widen to the west)

Estimated property requirements to accommodate standard County road cross-section and drainage/grading improvements:

- 3681 9th Line (agricultural)



Limit of existing road platform

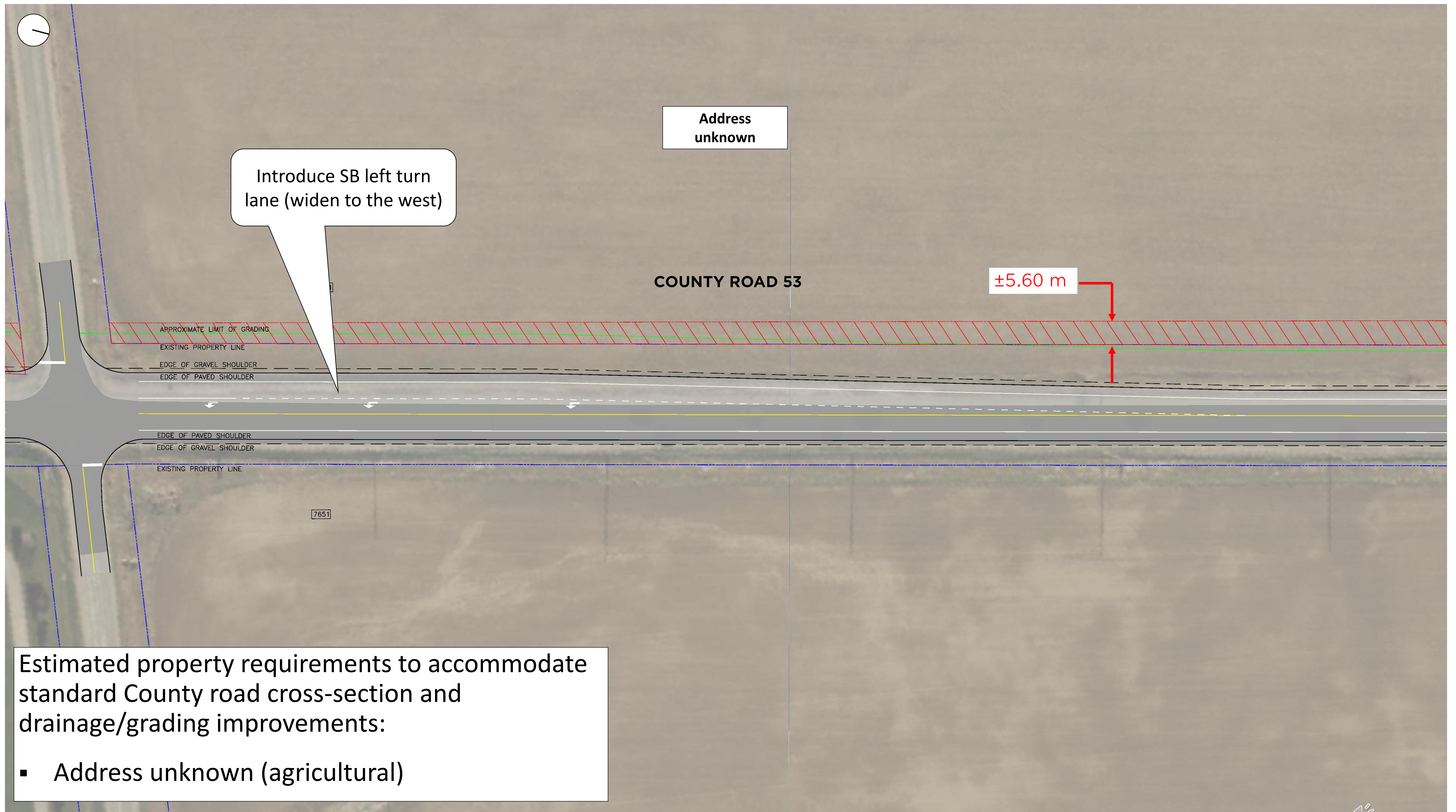


Limit of proposed widening



Approximate property requirements (to be refined during detail design)

RECOMMENDED SOLUTION – PROPERTY IMPACTS

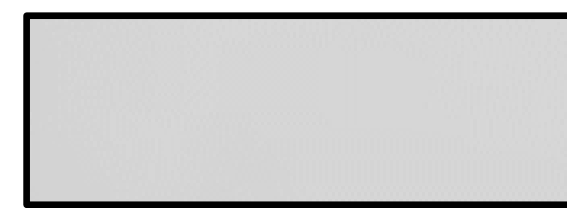


Estimated property requirements to accommodate standard County road cross-section and drainage/grading improvements:

- Address unknown (agricultural)



Limit of existing road platform

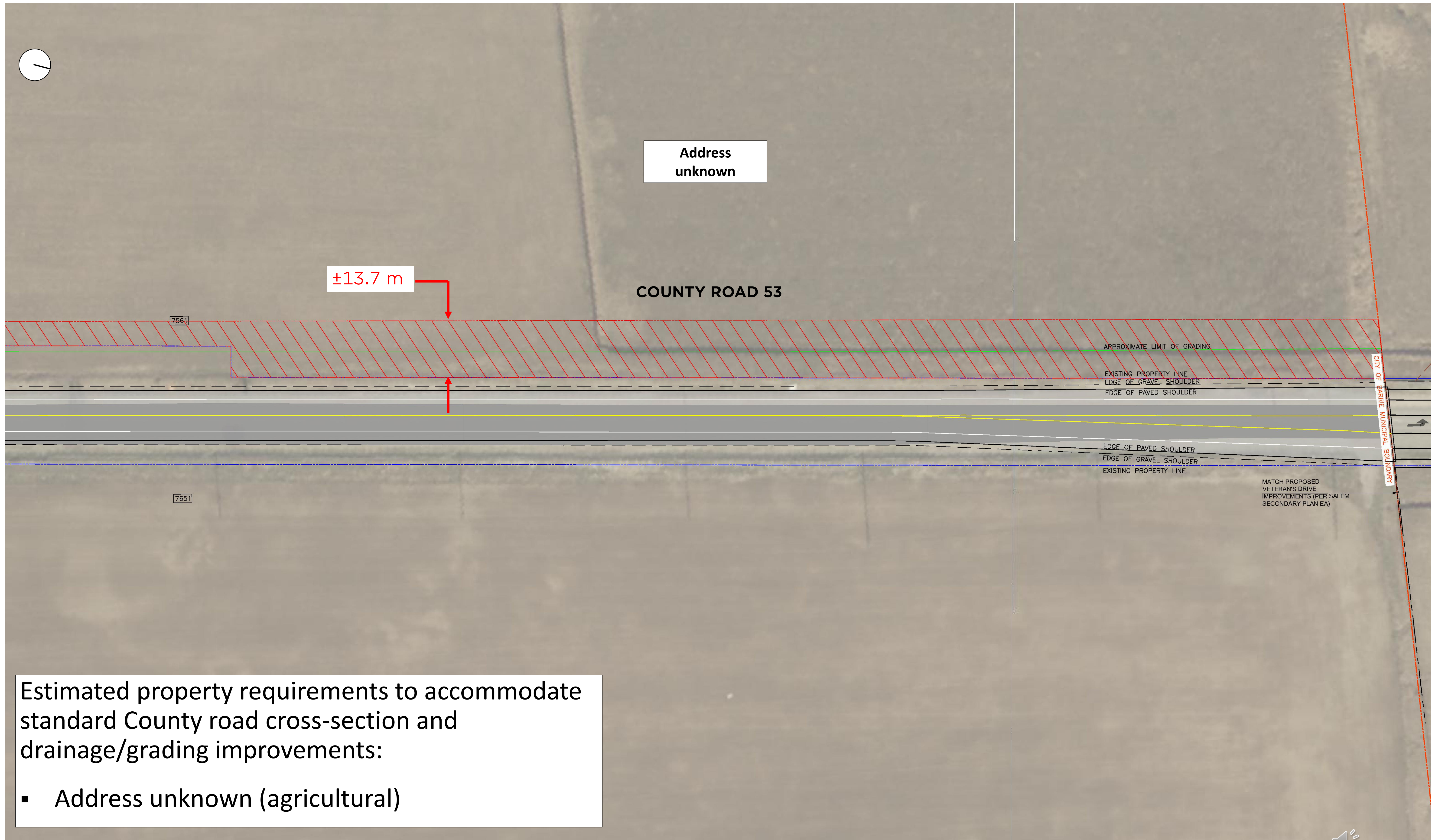


Limit of proposed widening



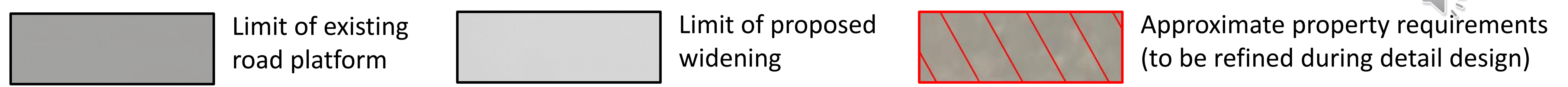
Approximate property requirements (to be refined during detail design)

RECOMMENDED SOLUTION – PROPERTY IMPACTS



Estimated property requirements to accommodate standard County road cross-section and drainage/grading improvements:

- Address unknown (agricultural)



RECOMMENDED SOLUTION – PROPERTY IMPACTS

4

NEXT STEPS



The following are available on the County's website:

- presentation
- comment sheet



Please submit any comments by
May 25, 2023



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WE ARE HERE



Phase 1

Phase 2

Phase 3

Phase 4

Phase 5

NEXT STEPS

Establish the Preferred Solution

Confirm Class EA Schedule



Schedule B
- Notice of Completion
- proceed to Phase 5

Identify & assess alternative designs for the Preferred Solution

Establish the Preferred Design

Prepare an Environmental Study Report

Design & Construction



THANK YOU

